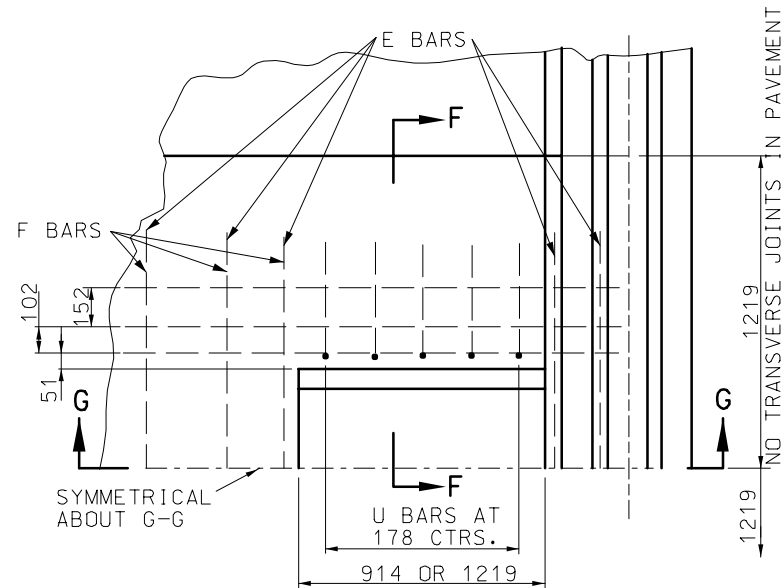
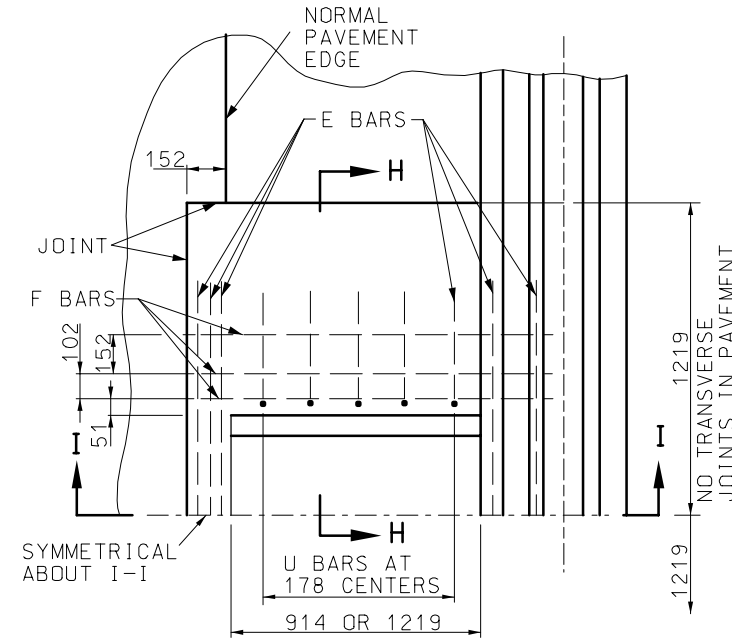


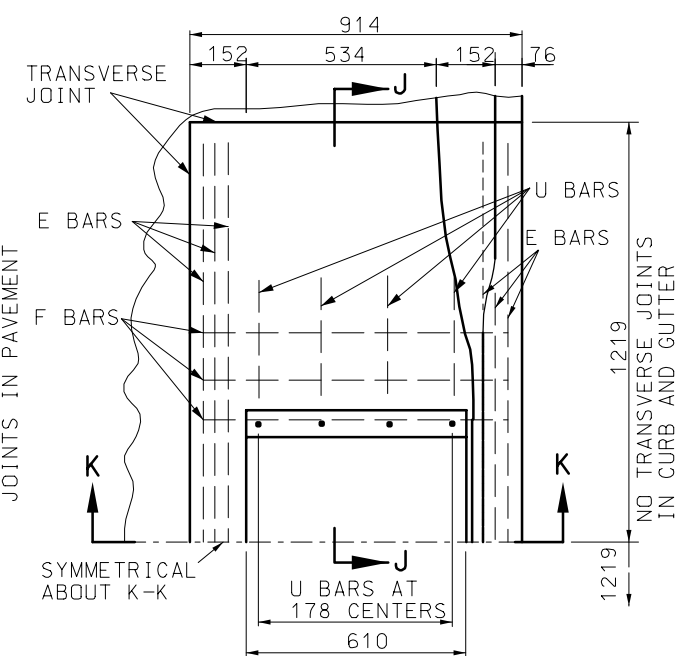
HALF-PLAN



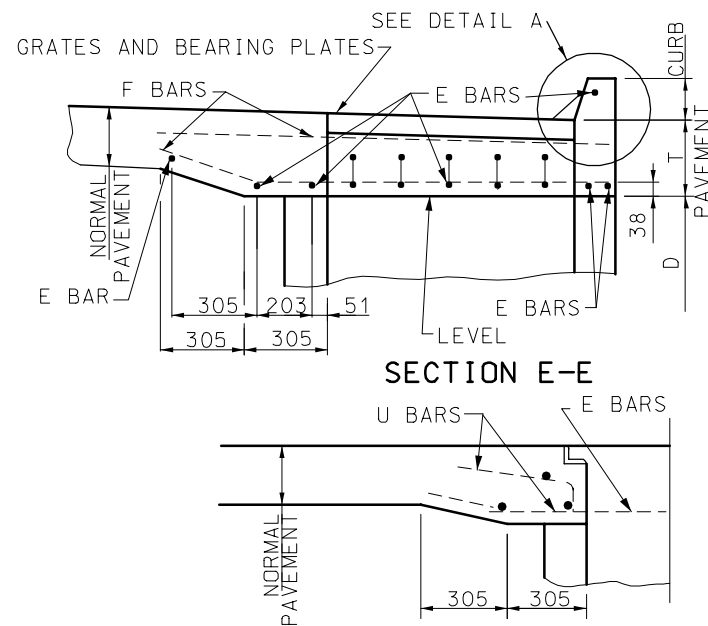
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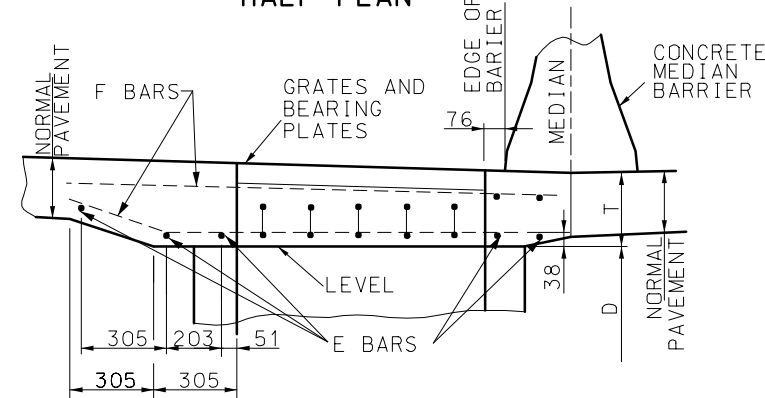
HALF-PLAN



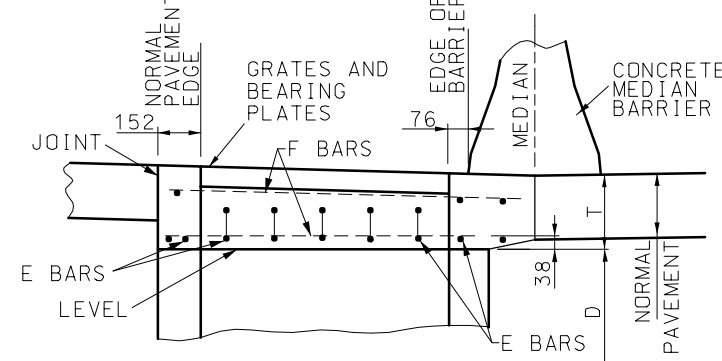
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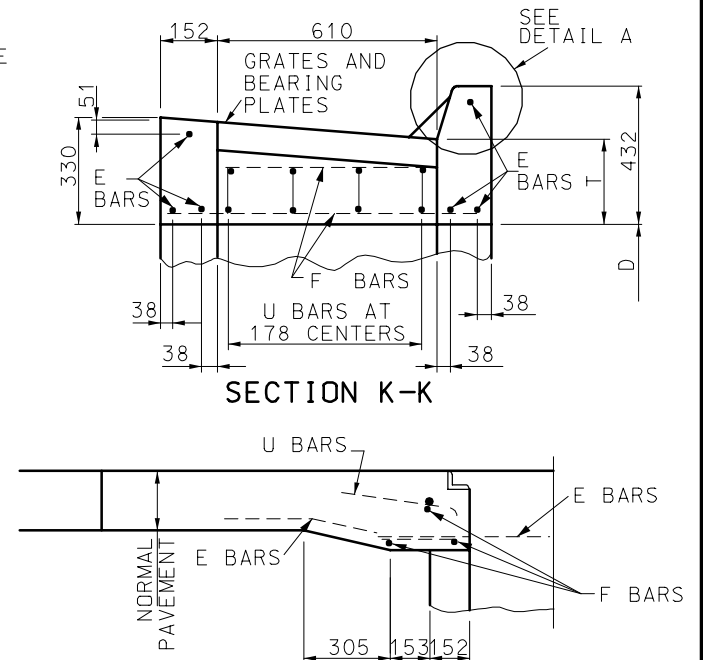
SECTION E-E



SECTION G-G

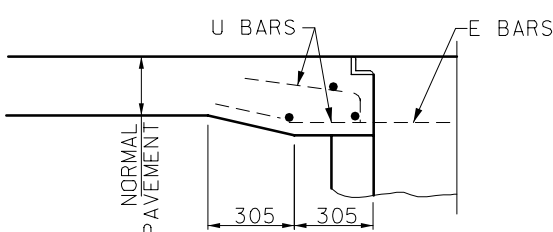


SECTION I-I

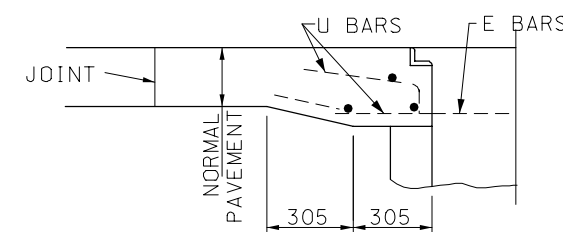


SECTION K-K

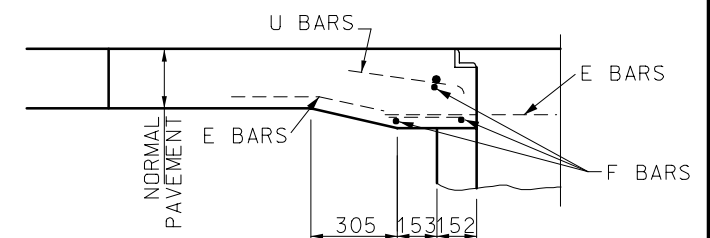
HALF SECTION D-D
TYPE A
INLET ADJACENT TO CURB



HALF SECTION F-F
TYPE B
DISTANCE BETWEEN BARRIER
AND PAVEMENT EDGE
GREATER THAN INLET WIDTH



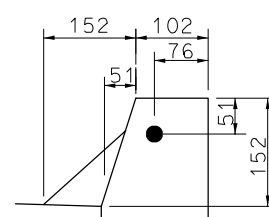
HALF SECTION H-H
TYPE C
DISTANCE BETWEEN BARRIER
AND PAVEMENT EDGE EQUAL
TO INLET WIDTH



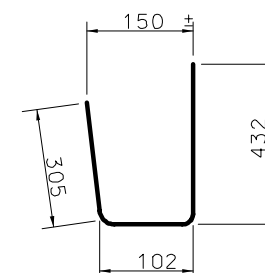
HALF SECTION J-J
TYPE D
CURB & GUTTER

PAVEMENT - BAR BILL				
TYPE	WIDTH	LENGTH	MARK NO.	LENGTH mm
A	914	610	E 6	1829
			F 6	1676
			U 10	838
			E 6	1829
			F 6	1981
B	1219	610	U 14	838
			E 7	1829
			F 6	1829
			U 10	838
			F 6	2134
C	914	610	U 14	838
			E 7	1829
			F 6	1295
			U 10	838
			F 6	1600
D	610	610	U 14	838
			E 6	2134
			F 6	838
			U 8	838

PAVEMENT THICKNESS	"T" DIMENSION
LESS THAN OR EQUAL TO 275	275
300	300
325	325
GREATER THAN OR EQUAL TO 350	350



DETAIL A

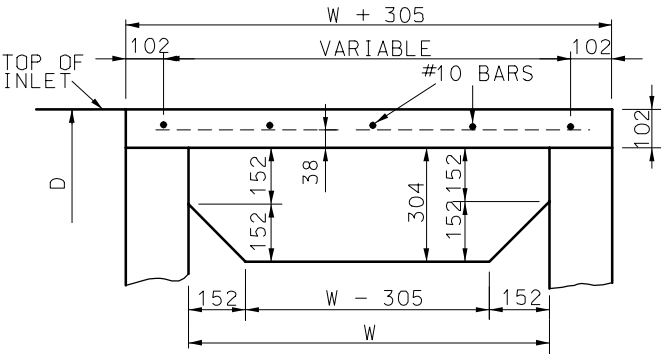


U-BARS
BENDING DIAGRAM

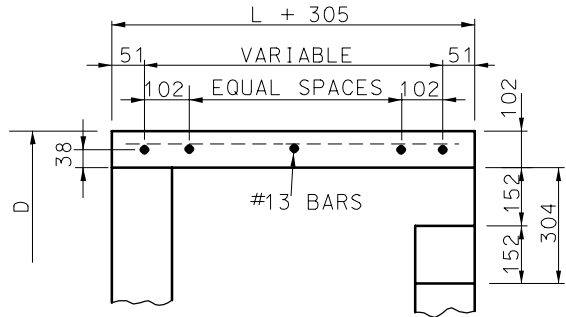
GENERAL NOTES:

ALL DIMENSIONS SHOWN ARE IN mm UNLESS OTHERWISE NOTED.

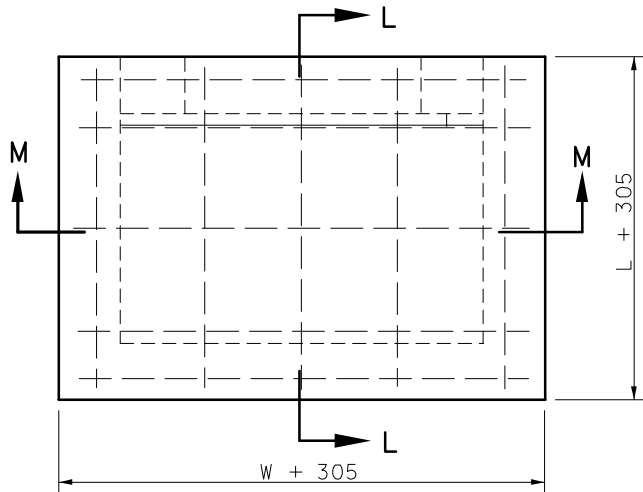
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION			
PRECAST DROP INLET COVERS			
DATE: _____	EFFECTIVE: 04-01-2005	M731.10Q	2 5



SECTION M-M



SECTION L-L



PLAN
TYPE E
PRECAST SLAB

TYPE E COVER BAR BILL				
WIDTH	LENGTH	BAR SIZE	NO.	LENGTH mm
914	610	#10	5	838
		#13	5	1143
914	914	#10	5	1143
		#13	5	1143
1524	610	#10	6	838
		#13	6	1753
1524	914	#10	6	1143
		#13	7	1753

BARS E, F, & U.....#13

GENERAL NOTES:

THE REINFORCEMENT SHOWN IS THE MINIMUM REQUIRED. AT THE CONTRACTOR'S OPTION, ADDITIONAL REINFORCEMENT MAY BE USED.

REINFORCING STEEL EDGE DISTANCE WILL BE 38 mm UNLESS OTHERWISE SPECIFIED.

NOT MORE THAN TWO LIFT HOLES OR LIFTING INSERTS MAY BE PROVIDED.

CLASS 3 EXCAVATION WILL BE PAID WITHIN VERTICAL PLANES 450 mm OUTSIDE OF THE OUTER WALLS OF THE BASE SECTION OF THE DROP INLETS. CLASS 3 EXCAVATION WILL NOT BE PAID FOR OUTSIDE THE FOOTING LIMITS.

GENERAL NOTES:

ALL DIMENSIONS SHOWN ARE IN mm UNLESS OTHERWISE NOTED.

THE SIZE OF THE DROP INLET AND TYPE OF COVER WILL BE SHOWN ON THE PLANS. THE CONFIGURATION DETAILS SHOWN ARE DESCRIPTIVE ONLY AND MAY BE VARIED TO CONFORM WITH ESTABLISHED MANUFACTUREING PROCEDURES. TOP OF DROP INLET WALLS SHALL BE CONSTRUCTED TO THE ELEVATION OF BOTTOM OF SLAB AT THE EDGE OF PAVEMENT OR BOTTOM OF CURB AND GUTTER AT DROP INLET.

WHERE THE DROP INLET IS LOCATED IN AN UNPAVED AREA, THE TOP OF THE DROP INLET WALLS SHALL BE SET TO THE ELEVATION SHOWN ON THE PLANS.

ALL CONCRETE ABOVE THE TAR PAPER SEPARATION JOINT IS TO BE CONSTRUCTED DURINGPAVING OPERATIONS OR CURB AND GUTTER CONSTRUCTION, AND WILL BE PAID FOR AS SQUARE METERS OF CONCRETE PAVEMENT OR METERS OF CURB AND GUTTER.

FORMED OR CUT-OUT OPENINGS SHALL BE PROVIDED WHERE PIPE INLETS AND OUTLETS ARE SHOWN ON THE PLANS.

REINFORCING BARS IN PAVEMENT SHALL BE EPOXY COATED AND SECURELY TIED TOGETHER AND FASTENED TO AVOID ANY POSSIBLE DISPLACEMENT DURING THE PLACING OF CONCRETE, REINFORCEMENT SHOWN IS IN ADDITION TO ANY REINFORCEMENT SHOWN FOR CONCRETE PAVEMENT OR CURB AND GUTTER.

JOINTS SHALL BE SEALED IN ACCORDANCE WITH SECTION 726.3.1 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR WILL BE PERMITTED TO CAST IN PLACE THE DROP INLETS CALLED FOR IN THE PLANS, TO THE DIMENSIONS REQUIRED FOR PRECAST DROP INLETS.

IF THE CONTRACTOR ELECTS TO CAST IN PLACE THE DROP INLETS, PAYMENT WILL BE MADE AT THE CONTRACT UNIT PRICE FOR PRECAST DROP INLETS.

WELDED WIRE FABRIC MAY BE USED IN LIEU OF REINFORCING BARS, THE REINFORCEMENT SHALL NOT BE LESS THAN 0.49 mm²/mm BOTH HORIZOTALLY AND VERTICALLY.

NO DIRECT PAYMENT WILL BE MADE FOR REINFORCING STEEL.

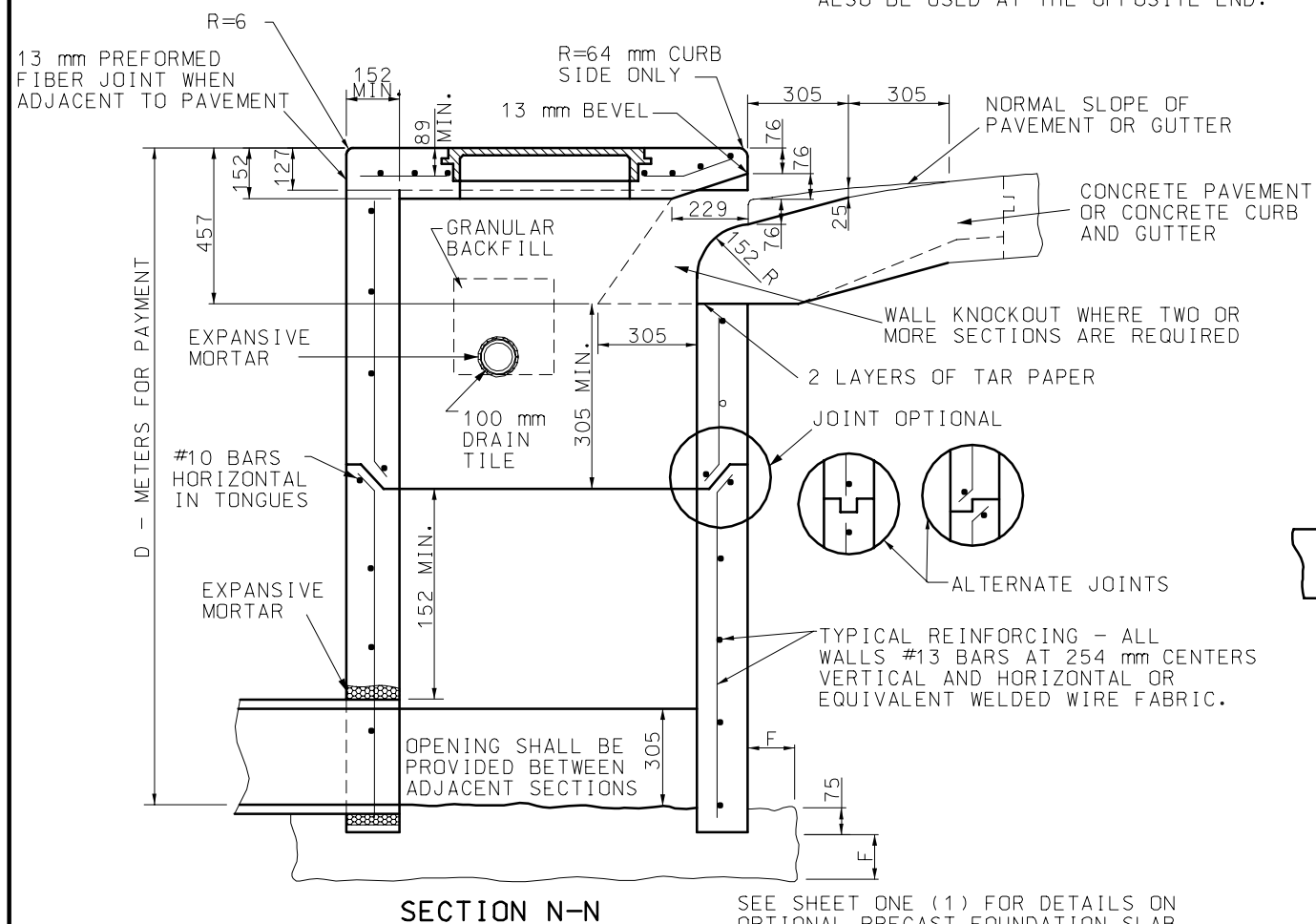
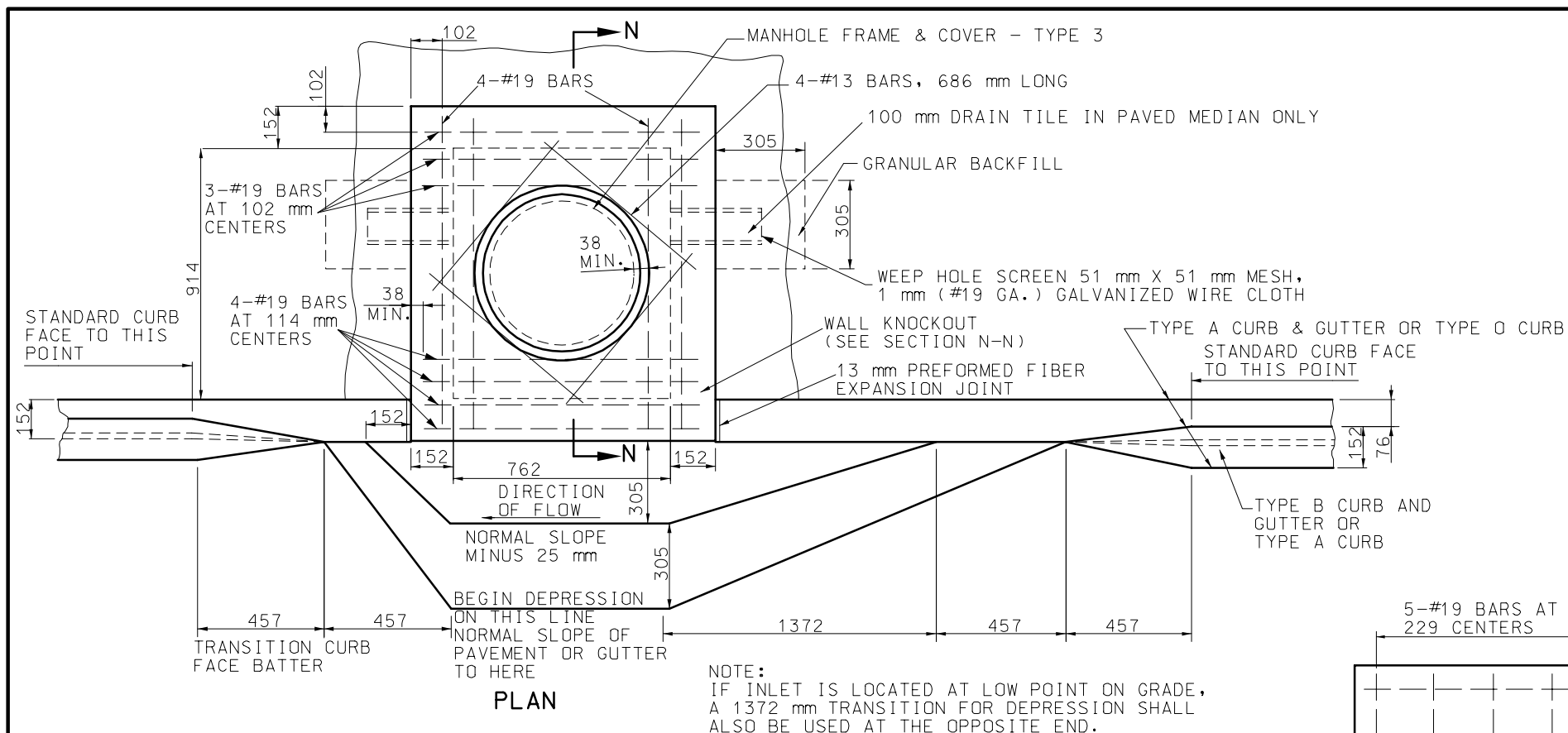
NO DIRECT PAYMENT WILL BE MADE FOR CUTTING PIPE NOR FOR CUTTING OR BENDING REINFORCING STEEL.

THE TOP OF INLET PIPES SHALL NOT BE SET BELOW THE TOP OF THE OUTLET PIPE.

USE CURB INLETS AT LOW POINTS.

NO DIRECT PAYMENT WILL BE MADE FOR FORMING FOR GRATES AND BEARING PLATES.

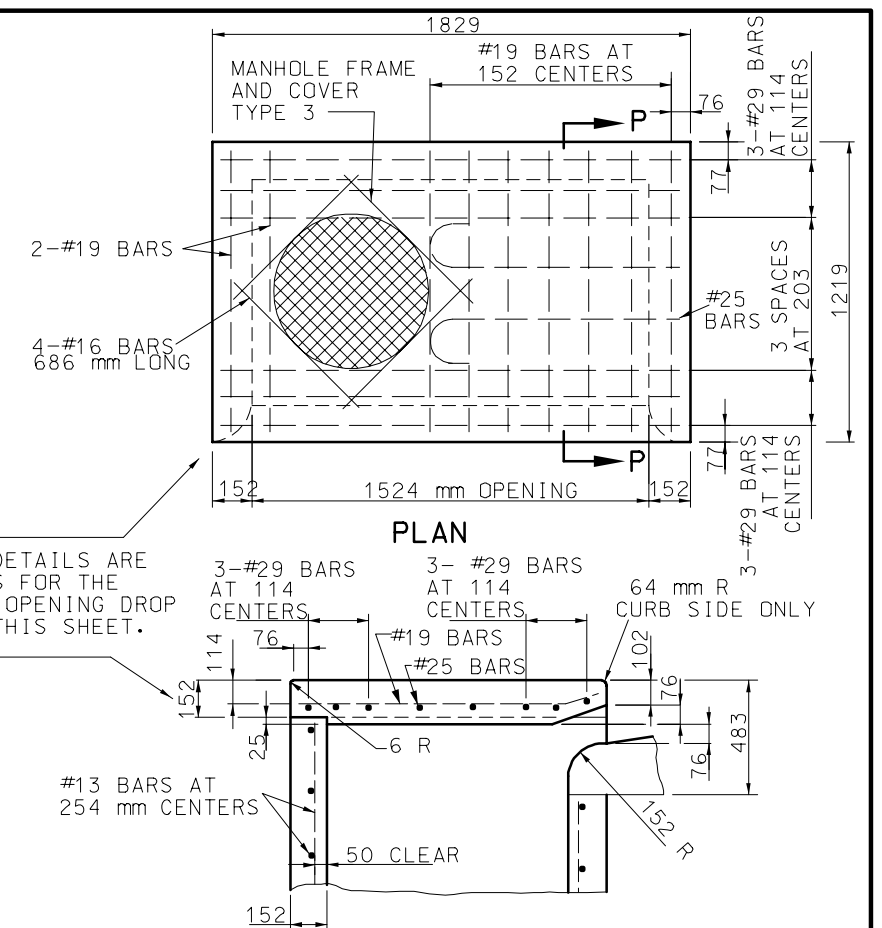
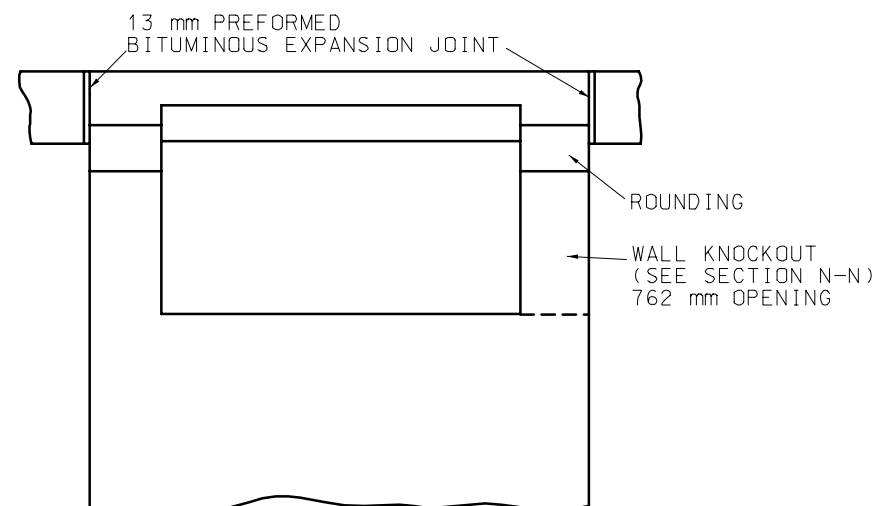
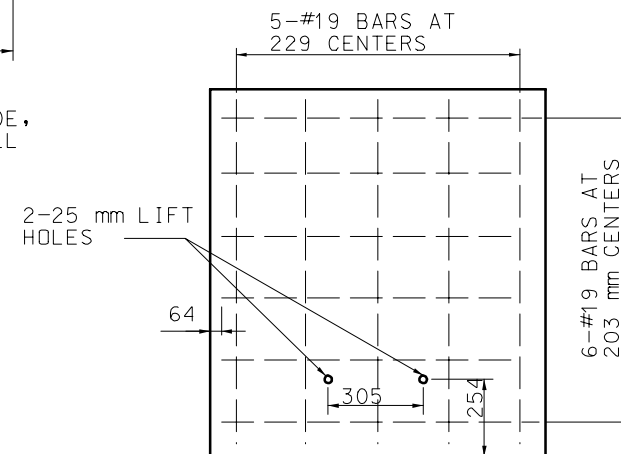
MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION			
		PRECAST DROP INLET	
DATE:_____	EFFECTIVE: 04-01-2005	M731.10Q	3/5



F = 150 mm FOR D ≤ 1829 mm.
F = 300 mm FOR D > 1829 mm.

SEE SHEET ONE (1) FOR DETAILS ON OPTIONAL PRECAST FOUNDATION SLAB.

PRECAST BASE SECTION SET ON CONCRETE BLOCKS. THE FOUNDATION SLAB AND THE INVERT SHALL BE POURED MONOLITHICALLY.



SECTION P-P OPTIONAL PRECAST CURB INLET 1524 mm OPENING

GENERAL NOTES:

ALL DIMENSIONS SHOWN ARE IN mm UNLESS OTHERWISE NOTED.

NOTES PERTAINING TO TYPE "T":

THE LENGTH AND DEPTH OF THE INLET SHALL BE AS SHOWN ON THE PLANS.

WALLS BETWEEN THE ADJACENT SECTIONS SHALL BE SEALED IN ACCORDANCE WITH SECTION 726.3.1 OF THE STANDARD SPECIFICATIONS.

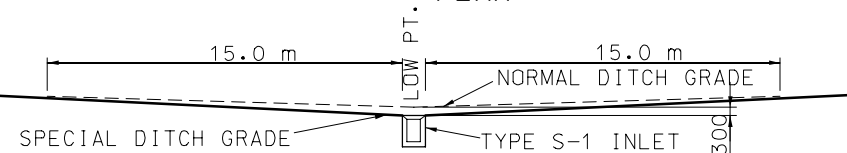
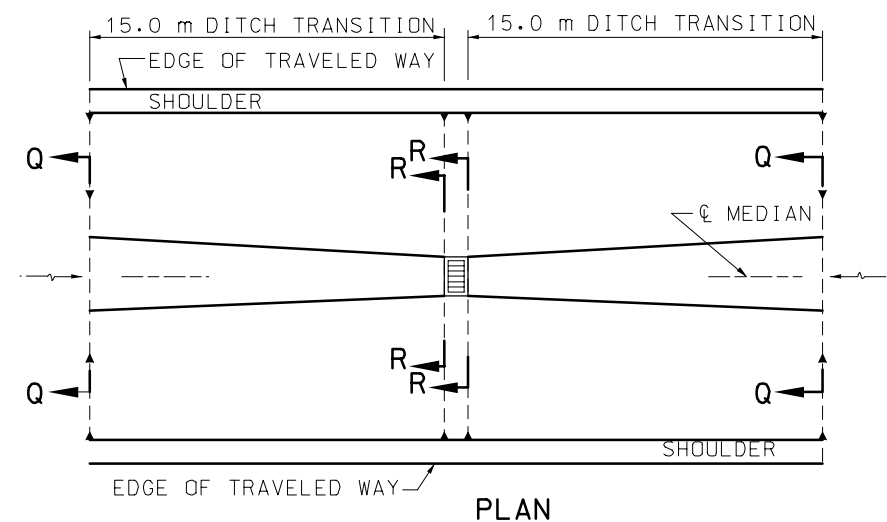
IF DEPTH OF INLET EXCEEDS 1829 mm THE PRECAST UNITS MAY BE FURNISHED IN TWO OR MORE SECTIONS.

IF TWO OR MORE SECTIONS ARE USED THE TYPE 3 MANHOLE FRAME AND COVER SHALL BE IN THE DOWNSTREAM SECTION ONLY.

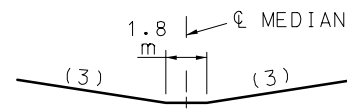
IF A 1524 mm OPENING IS REQUIRED, TWO 762 mm OPENING SECTION OR ONE 1524 mm OPENING SECTION MAY BE PROVIDED AT THE CONTRACTOR'S OPTION.

SEE SHEET 1 FOR STEP DETAILS AND SHEET 2 FOR GENERAL NOTES.

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION			
PRECAST DROP INLET CURB INLET - TYPE T			
DATE: _____	EFFECTIVE: 04-01-2005	M731.10Q	4 5

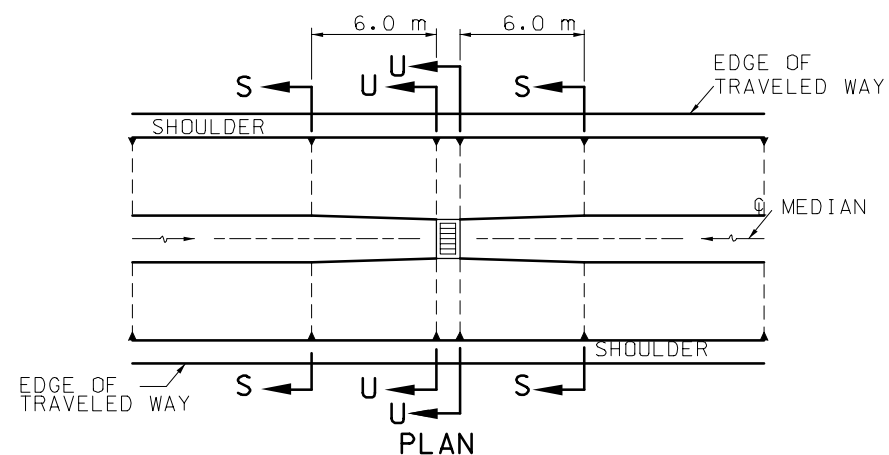


SECTION ALONG CENTERLINE OF
MEDIAN RELIEF AT LOW POINT



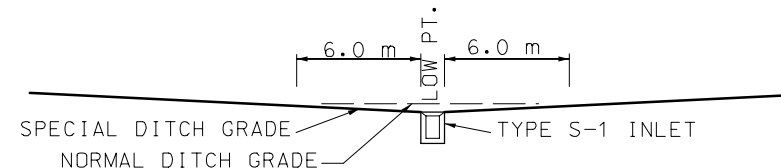
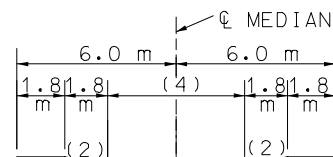
SECTION R-R

- (1) DITCH WIDTH AS GIVEN ON TYPICAL SECTION.
- (2) DITCH SLOPE AS GIVEN ON TYPICAL SECTION.
- (3) VARY SLOPE TO ACHIEVE MINIMUM 150 mm FREEBOARD ABOVE TOP OF INLET GRATE ON GRADE, OR 300 mm ADDITIONAL DITCH DEPTH BELOW NORMAL DITCH GRADE AT LOW POINT.
- (4) VARY WIDTH TO ACHIEVE MINIMUM 150 mm FREEBOARD ABOVE TOP OF INLET GRATE.



PLAN

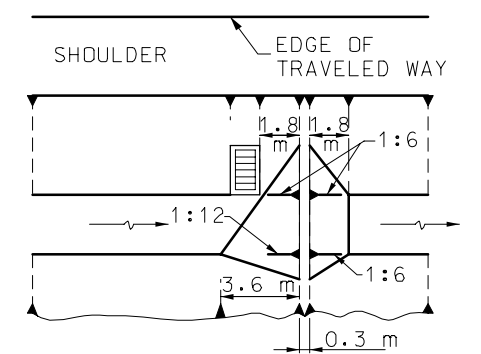
PLAN

SECTION ALONG CENTERLINE OF
MEDIAN RELIEF AT LOW POINT

SECTION T-T

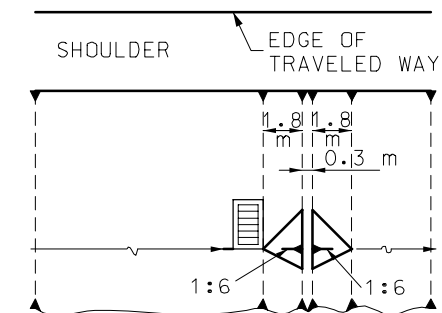
Diagram illustrating the plan view of a 12.0 m wide road. The road is divided into a central 3.0 m median and two 4.5 m lanes. The road has a 1.8 m shoulder on each side. The median is labeled "MEDIAN" and the shoulder width is labeled "1.8 m".

SECTION U-U

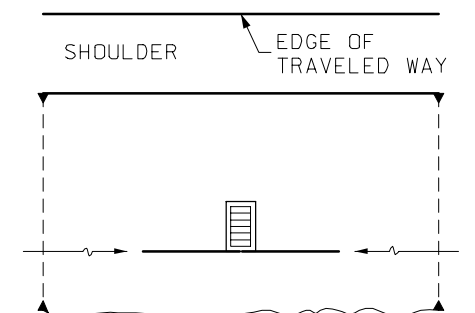


TYPE S-3 INLET IN ROADWAY
DITCH (FLAT DITCH ON GRADE).

PLAN 2



PLAN 3



TYPE S-3 INLET AT LOW
POINT (V DITCH).

PLAN 5

GENERAL NOTES:

ALL DIMENSIONS SHOWN ARE IN mm UNLESS OTHERWISE NOTED.

THIS DRAWING IS FOR GENERAL INFORMATION ONLY. ACTUAL CONSTRUCTION
DETAILS SHALL CONFORM TO THOSE SHOWN ON THE DETAILS PLANS.

DETAILS ON THIS SHEET ARE ONLY FOR USE WITH STRUCTURES LOCATED IN THE
MEDIAN OR WITHIN THE CLEAR ZONE.

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION			
	<p align="center">PRECAST DROP INLET TYPICAL LOCATION DETAILS - TYPE S</p>		
DATE: _____	EFFECTIVE: 04-01-2005	M731.10Q	<div>5</div> <div>5</div>